Dr. Esmaeil Tekyehsadat
Director of Transport and Communication ECO secretariat

ECO Corridors;
Best way for Connectivity between market access and market Competitiveness

WP30,131session 12-15 June 2012

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I – ECO Corridor **Priorities**

1. **Choosing the "right" corridor**
2. **Maximize rider ship and opportunities**
3. *Initiation elements for Transit Operation*
4. **Elements eligible for funding**

1- "right" corridor

For this we got regional support and momentum for transit

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2- Maximize rider ship in Corridor network

For this purpose we focus on connections among stations along the corridor

3- Initiation elements for Transit Operation

For this purpose we operate test run of Truck and Container Train and Regular run of the Truck and Container Train

And

Transit Mechanisms

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4- Elements eligible for funding

For this purpose we focus on efficient involvement of public and private partnership

ECO Corridors

- Islamabad- Tehran- Istanbul Corridor (ITI)
- Iran-Afghanistan- Tajikistan- Kyrgyzstan Corridor (KIAT)
- Kazakhstan- Turkmenistan- Iran Corridor
- Almati- Bandarabas Corridor
- Almati – Istanbul Corridor
- Rasht- Astara- Baku Corridor
- Follow up initiative for construction standard Rail among Iran- Afghanistan- Tajikistan- Kyrgyzstan to China

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Maximize rider ship in Corridor network

- For maximizing rider ship in ECO Corridors, we are focusing on connectivity among :

- Border stations ,

- Multimodal Terminals and,

- Accessible ports

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Initiation elements for Transit Operation Test Run and Regular Run of Truck and Container Train

- Truck Caravan 2010

- ECO RMT Project 2011

- Bandar Abas- Almati demonstration of Container Train 2011

- TIR Test run of Truck with Afghanistan 2012

- Islamabad- Tehran – Istanbul Regular Container Train

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Initiation elements for Transit Operation Transit Mechanisms

- Implementation of TIR in Afghanistan
- Accession of Pakistan to TIR
- Joint workshop with UNECE on Unified Rail Law
- Planning for Joint Workshop with UNECE on Harmonization Convention
- Using UNCTAD potential on C2C Networking
- Initiating ECO Uniform Visa Sticker

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Elements eligible for funding

- ECOLPAF Initiatives
- Initiating Good Practices with IDB
- Using potential of other International Organizations ADB, WB, IMF
- Initiating Public – Private Conferences , Bandarabas Conference , Istanbul TIR Conference
- Initiating joint action course with IRU, UIC
- Initiating ECO Bank for project
- Using potential of our member states for ECO Project
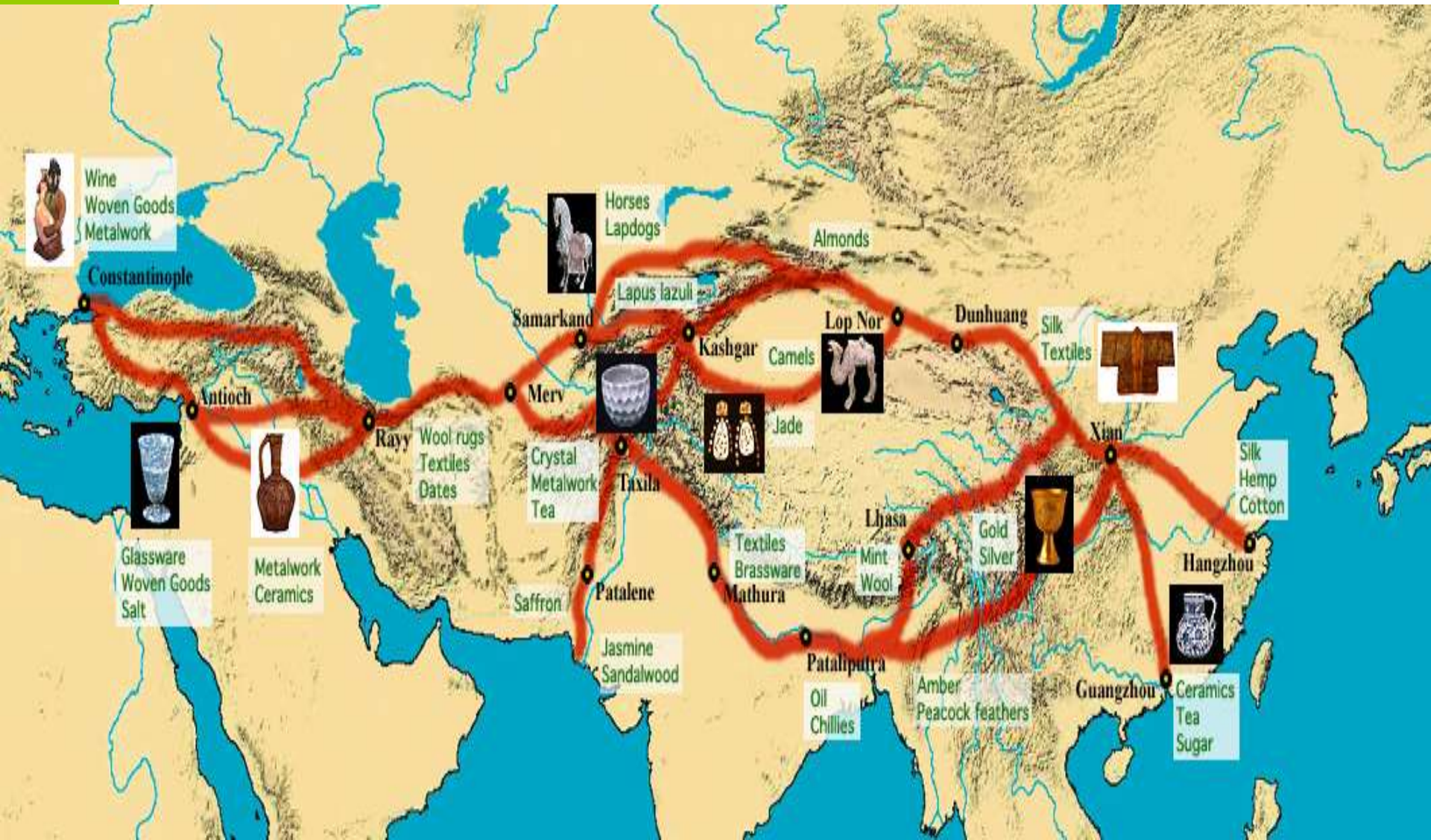- Planning for ECO Business Transit FORUMS

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Map of [Marco Polo](#)'s travels in 1271–1295
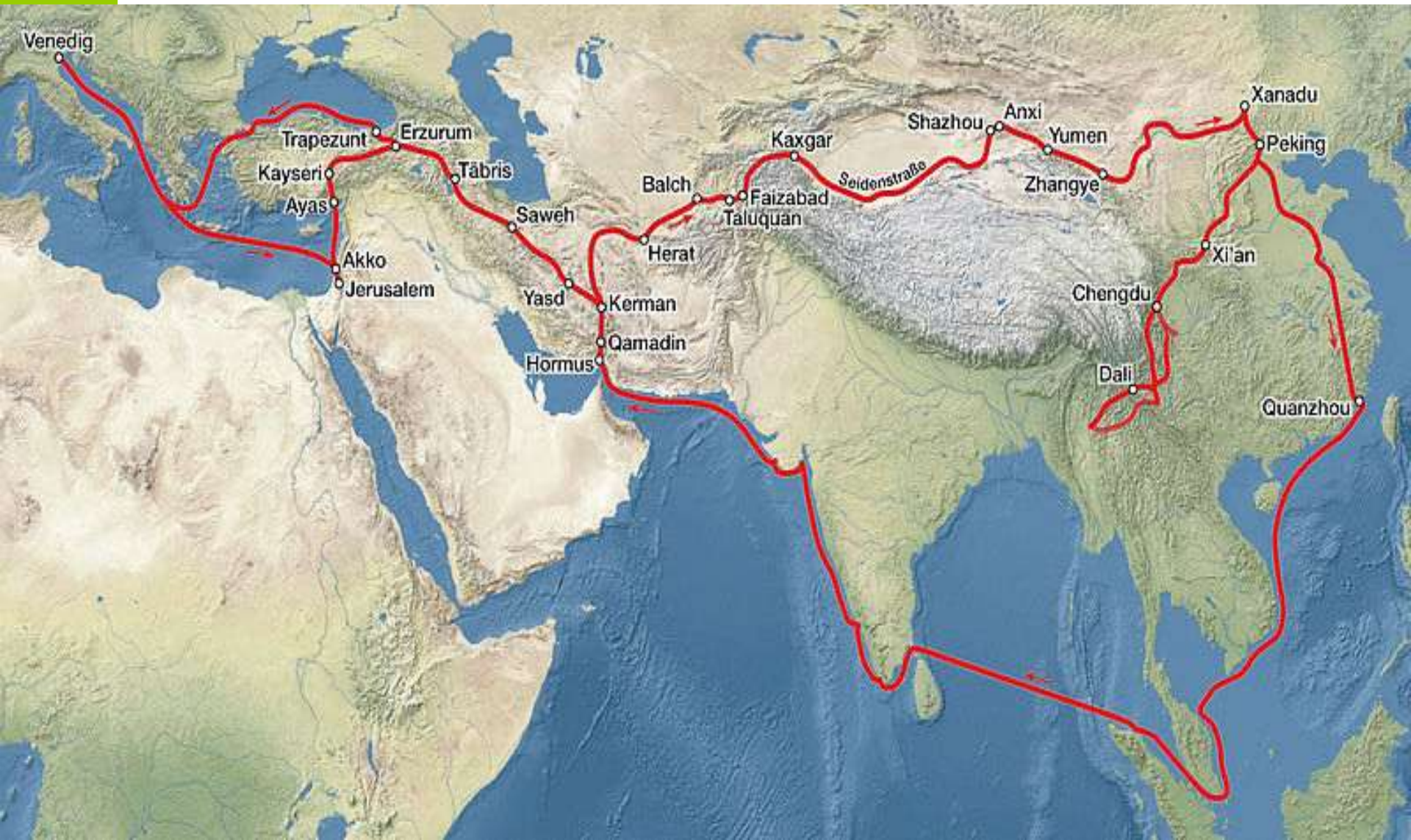
Travels of Marco Polo

Silk Road Trade Routes

Route of Silkroad

The Map of Alignment and Routes
in Initiative construction of standard Rail between Iran, Afghanistan, Tajikistan, Kyrgyzstan

Kyrgyz - Osh-Batken-Isfana Road

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NIZHNIJ PJANSH BORDER TERMINAL

12/6/2012

Details of Gradients

Sl. No.	Location Of Station	Distance(KM)	Elevation
1	Sherkhan Bandar	0	330
2	Kunduz	75	371
3	Khulm	188	395
4	Naibabad	217	369
5	Mazar-e-Sharif	244	357
6	Aqcha	350	334
7	Sheberghan	403	330
8	Andkhoy	475	301
9	Aqina	510	250
11	Almar	643	925
12	Qaiser	698	1234
13	Ghormach	743	853
14	Murghab	787	473
15	Qala-e-Naw	903	887
16	Kushki Kuhna	953	1243
17	Kushk	1024	853
18	Torghundi	1073	670
19	Herat	1094	968

Rasht- Astara- Baku Construction of Rail

Azerbaijan Rail

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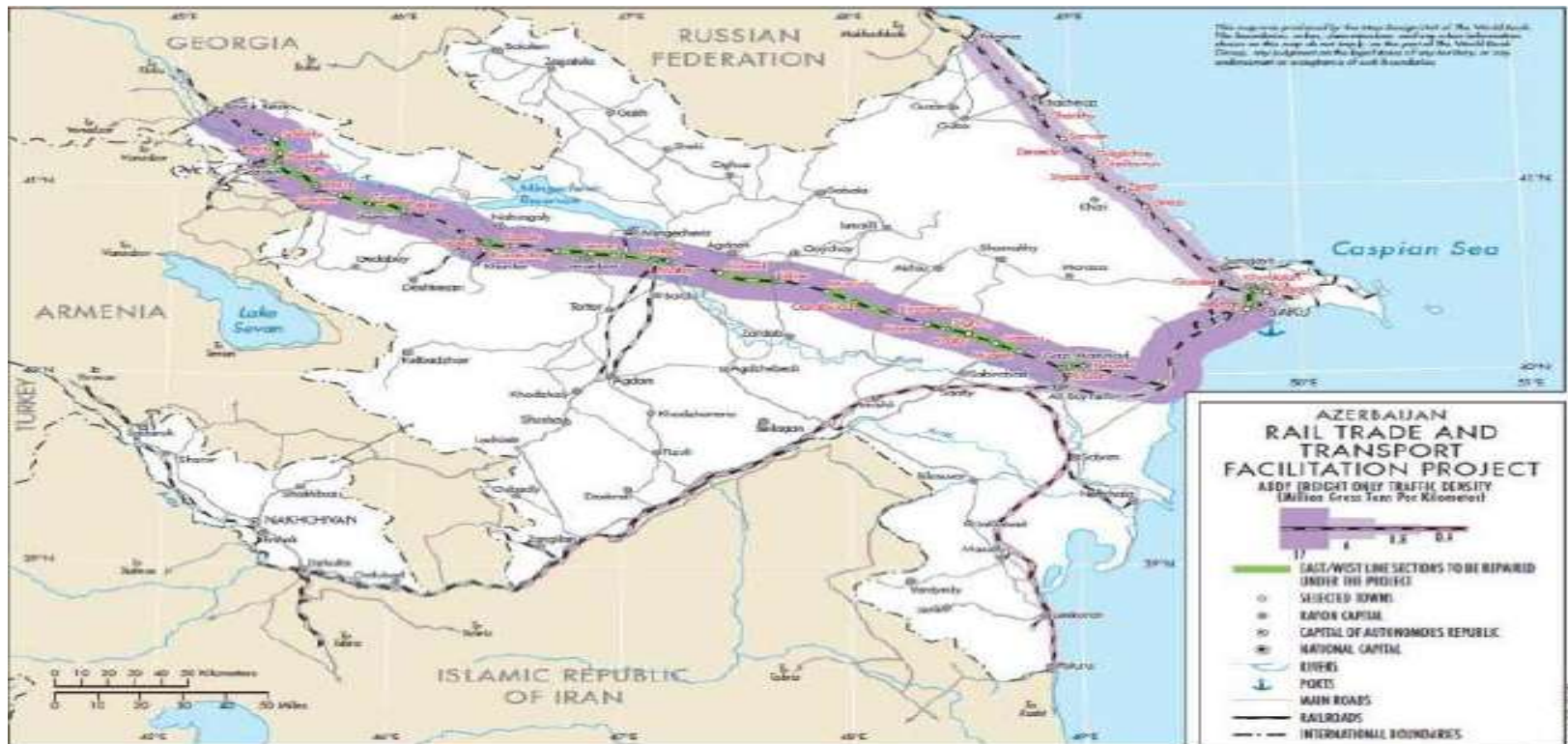
ALYAT SEA TRADE PORT LOGISTICS CENTRE

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ECO ITI Corridor

REFAHIYE JUNCTION-ERZURUM-GÜRBULAK ROAD UPGRADING

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ECO Almaty – Istanbul Contain Train

BEINEU-SHALKAR NEW RAILWAY

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NAVOI AIRPORT UPGRADING

12/6/2012
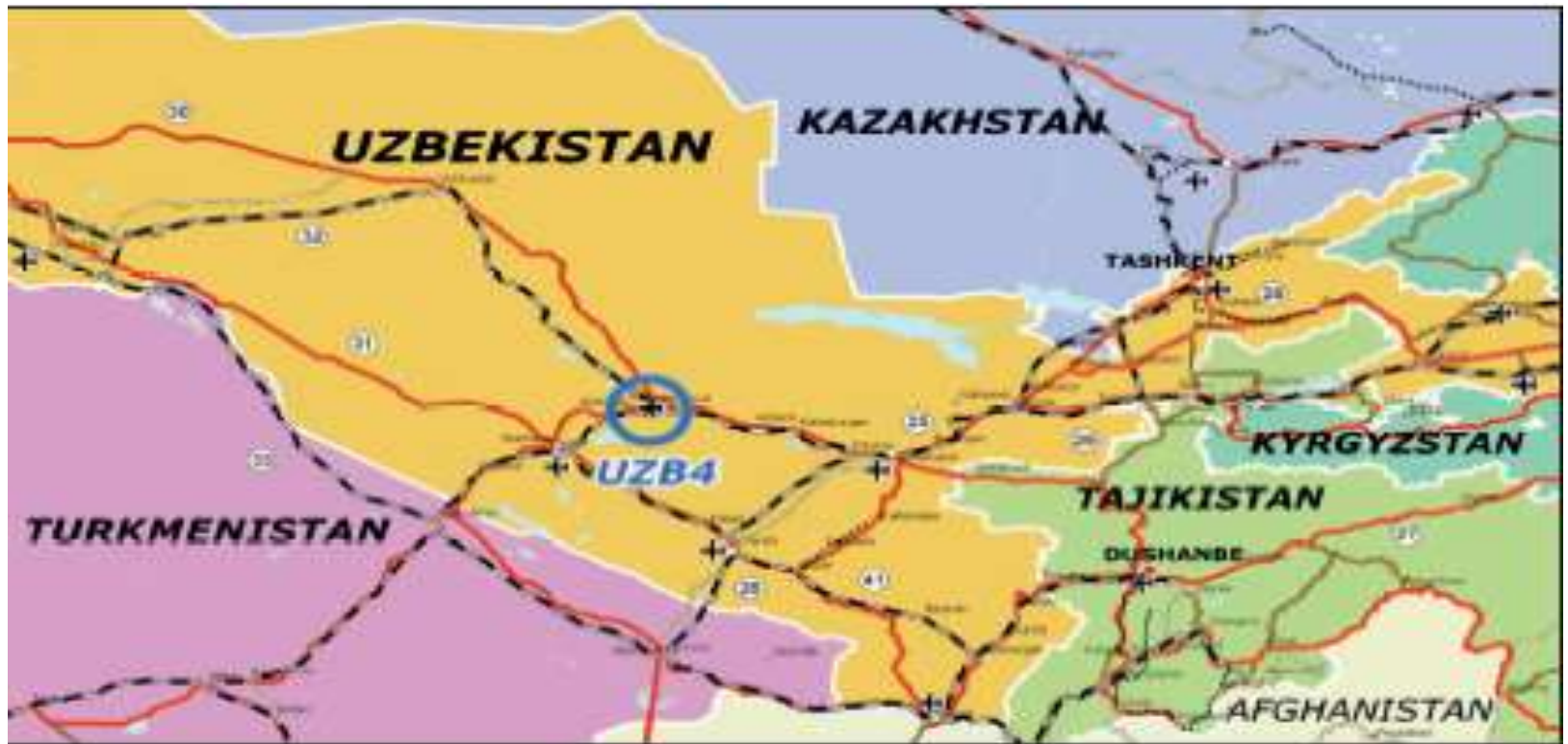
Almaty – Istanbul
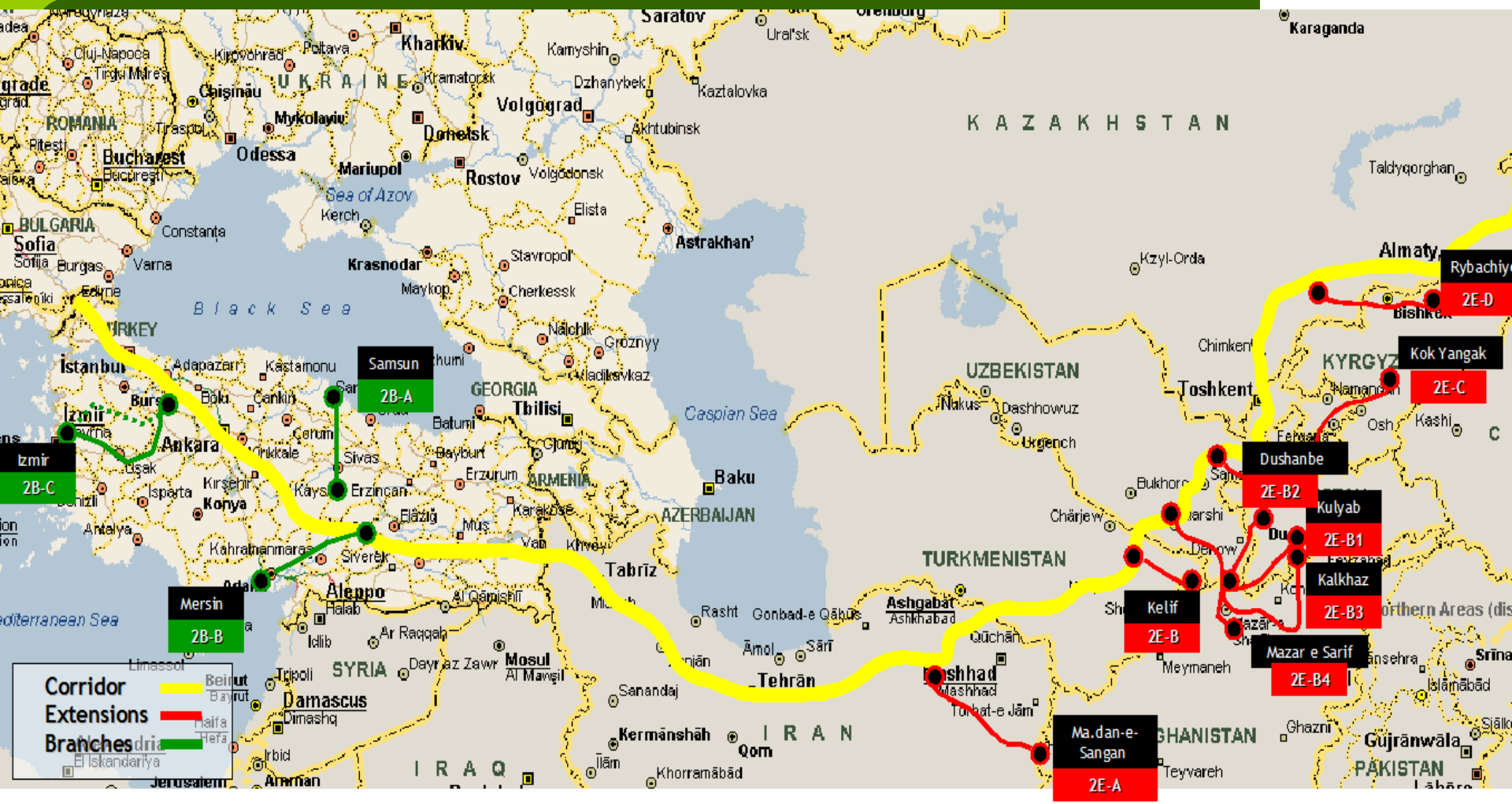
ECO Kazakhstan- Turkmenistan – Iran Construction of Rail

ECO and Africa

Mineral Resource Development in SADC Countries

Source: Conference for Ambassadors in Middle East Region, 2009

12/6/2012

Thanks