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**Economic Commission for Europe****Inland Transport Committee****Working Party on Customs Questions affecting Transport****128th session**

Geneva, 7 – 10 June 2011

Item 9 (b) (ii) of the provisional agenda

**Customs Convention on the International Transport of Goods****under cover of TIR Carnets (TIR Convention, 1975) –****Revision of the Convention: Amendment proposals for the Convention****Proposals to introduce a code system for reporting defects in load compartments of vehicles used for the TIR procedure****Note by the secretariat****I. Background**

1. At its 127th session, the Working Party or WP.30, inter alia, discussed amendment proposals to Annex 3 of the TIR Convention, submitted by the European Union (EU) (ECE/TRANS/WP.30/2010/12), with the aim to resolve practical problems in using Certificates of Approval when Customs authorities find an approved vehicle to have a major defect and make the appropriate inscription in item No. 10 of the Certificate of Approval. As the space reserved for the text is rather limited, such defect is not always properly described. In addition, the Customs authorities use their own national language and handwriting to report defects. As a result, the carrier or competent authorities in another Contracting Party may not be able to read the inscription. To improve the situation, the European Union proposed introducing a uniform system of codes to record defects in the Certificate of Approval.

2. WP.30 generally supported these proposals and pointed out that the list of defects in document ECE/TRANS/WP.30/2010/12 may need to be checked for integrity and consistency. Delegations were invited to study this list in detail and to send their comments to the secretariat by 15 March 2011.

3. In this document, the secretariat reproduces the comments received by 15 March 2011 for consideration by the Working Party. Comments have been submitted by the Governments of Estonia and Turkey. For the sake of convenience, the secretariat has regrouped the comments according to their appearance in the European Union document.

For the sake of convenience, the code system, as proposed in document ECE/TRANS/WP.30/2010/12, is reproduced in the Annex to this document.

## II. Comments/Questions

Paragraph 11:

- (a) Why does the code numbering start with 02 instead of 01?
- (b) Codes 03–06 should be covered by a single code. While the exact location is already specified and there is a reference to an error, identification of this error would not cause any difficulty. A national competent authority will decide on how to rectify the error.
- (c) Code 08: same as above. Reference to a thong would be sufficient. Thus, codes 07 and 08 might be covered by a single code.
- (d) Code 17: What is the meaning of this code? Does it mean that it is possible to open the load compartment without breaking a Customs seal or are the eyelets, referred to under 02 broken?
- (e) Code 18: same as above. What does it mean that sealing is not possible? Is it possible to open a valve, stopcock, etc., without breaking a Customs seals or what else? If it is definitely not possible to seal the load compartment, then the Certificate of Approval should not be issued at all and Code 20 should apply.
- (f) Code 20: The necessity of renewal should be specified. Does this mean that the paper of the Certificate of Approval is simply worn out, or that the period of validity has expired, requiring a renewal of the Certificate?
- (g) Code 21: This code is not clear. Does it mean that no photos are attached to the Certificate of Approval?
- (h) Code 24: A reduction of the number of codes referring to the sheet (codes 22–24) should be considered.
- (i) What should be done in case of any defect not covered by any of the other codes? Code 28 does not provide any clarification with regard to the nature of the defect.
- (j) The following amendments to the code list are proposed:
  - (i) Any defect on the exterior of the tractor, trailer or semi-trailer;
  - (ii) Inconsistency between the information available in the Certificate of Approval (colour, paint, company name, etc.) and the appearance of the vehicle;
  - (iii) Loss in endurance of the support unit of the sheet or bend/curve due to an accident or the improper loading of the consignment;
  - (iv) Disfunction of the bobbin of the round-up for sliding sheets.

## Annex

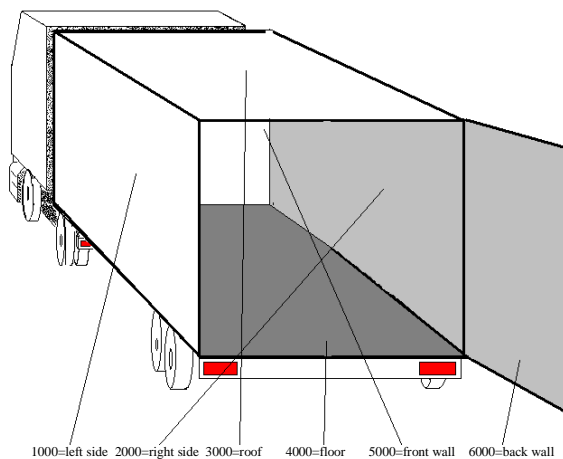
### Code system to be used to report defect remarks in the certificate of approval

The uniform system to be implemented could consist of four (4) digits code.

The code could divide the load compartment into six separate sections: left side, right side, floor, roof, front wall and back wall. In addition, the load compartment could be divided into three separate parts longitudinally (direction of motion): front, centre, back. It is considered not necessarily to divide front wall and back wall into separate parts since the area to be examined is quite small.

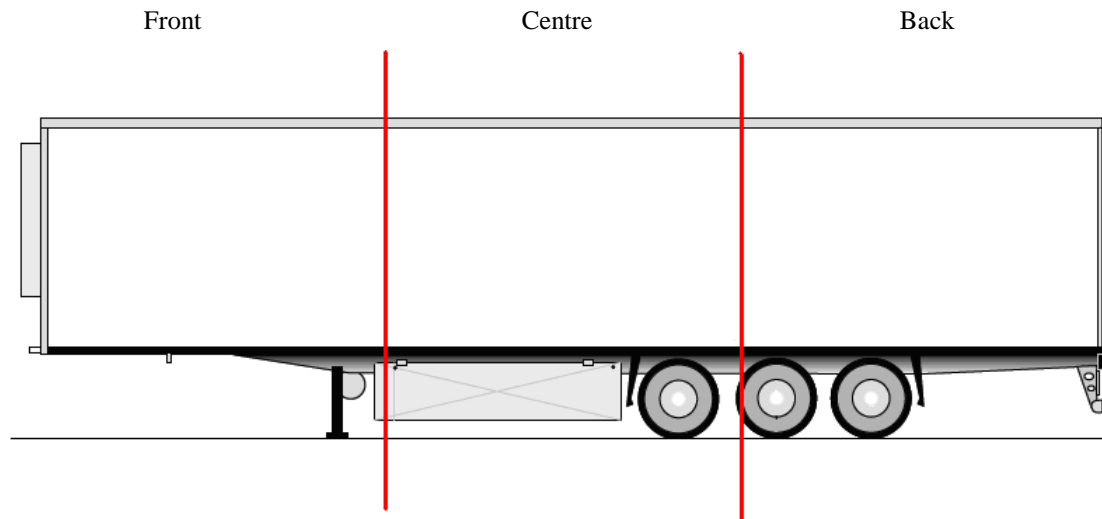
#### A. First number indicates the part in question of the load compartment

- 1000 Left side
- 2000 Right side
- 3000 Roof
- 4000 Floor
- 5000 Front wall
- 6000 Back wall
- 7000 The defect in question concerns the load compartment as a whole (e.g. a TIR plate is missing)



#### B. Second number indicates the part in question longitudinally

- 0100 Front (e.g. 1100 = left side, front)
- 0200 Centre
- 0300 Back

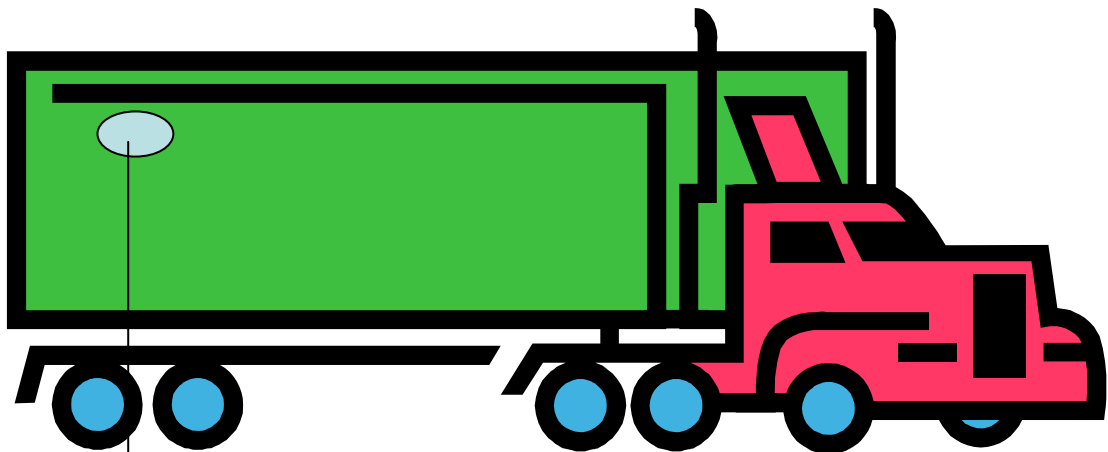


### C. Last two numbers indicate the defect itself

The following are the most common defects detected in practice:

- 02 End-piece of the sealing wire to be replaced
- 03 Space between securing rings/eyelets is too long
- 04 Reinforcement of the securing ring missing/to be patched up
- 05 Securing ring to be fastened from inside
- 06 Securing ring to be replaced
- 07 Thong to be replaced
- 08 Fastening of the thong to be repaired
- 09 Securing ring/eyelet missing
- 10 Eyelet model used at the upright incorrect
- 11 Floor fastening to be made from inside
- 12 Hole
- 13 Sliding sheet, fastening of the guidance bar insufficient
- 14 Sliding sheet, fastening insufficient
- 15 Strap/hinge fastening insufficient/to be secured
- 16 Locking device insufficient/to be secured
- 17 Sealing not possible
- 18 Stopcock/valve/flange/manhole-cover (sealing not possible)
- 19 Apertures for ventilation and drainage device not secured
- 20 Certificate of approval shall be renewed
- 21 Vehicle cannot be identified from the Certificate of Approval

- 22 Sheet to be repaired from inside/accordingly
- 23 Overlapping of the sheets insufficient
- 24 Sheet to be tightened accordingly
- 25 Securing ring fastened in impermanent way
- 26 Not allowed "blind" rivet used
- 27 Customs rope broken/worn through/too short/too long
- 28 Other defects



2312 = Hole in the back section of right side of semi-trailer